



# THE SEMAPHORE

NOVEMBER 2013

FOX VALLEY DIVISION • MIDWEST REGION



## Layout of the Month: David Crement's EBT

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East Broad Top Mikado #18 exits the Wray's Hill Tunnel on MMR David Crement's, finely crafted and prototypically inspired On3 EBT. Layout photos by Walt Herrick.

### December Article Help!

We want to run a "Trains in My Holidays" article in the December Semaphore. But we are in urgent need of your holiday train photos, and info to do so. Contact Walt Herrick with anything you may have. His contact info is page 11. Thanks!

MMR, David Crement's, East Broad Top Railroad layout is no stranger to FVD and NMRA members. His layout and superb modeling have been featured in past Semaphores, the FVD web site, NMRA Magazine (called Scale Rails when his layout article ran in July 2010), and EBT publications. He is a regular NMRA and Friends of EBT clinician. David is not just a model railroader, he is a dedicated EBT historian interested in preserving the railroad's history. His On3 layout closely follows the EBT's actual line from Saltillo to Woodvale, in Pennsylvania, and his models are scale replicas of EBT structures, locomotives, rolling stock and train operations. David's layout reflects 33 years of rail fanning and researching, and then careful planning and skilful building of a 15 mile section of his beloved EBT. On the next four Semaphore pages we welcome you to the East Broad Top R.R. in the fall of 1939.

## Jim Allen's Fun Stuff



Nope, this tank car wasn't in a wreck, nor was a heavy weight dropped on its single dome. It's just one weird looking (at least from an American perspective) foreign tank car used to haul vegetable oil. Its weathering looks pretty nice, though.

## David Crement's Layout of the Month



The arrival of the early morning miner's train led by Mikado #14 does not disturb the fellow reading a newspaper at the Robertsdale depot one bit. Robertsdale is a company town, and the tan field stone company store and red company houses can be seen in the background. (above) After a brief station stop, the train passes Mine #1 on its way south to drop off miners at the eight other mines the train serves. (left)



This man has a bird's eye view of #14's departure from his seat high atop the roof of Mine #1's tippie.



## David Crement's Layout of the Month con't



With a loaded coal train in tow, EBT #18 pokes its nose out from a forest of colorful fall trees and evergreens. The backdrop in this scene was hand painted by David's best friend, the late Mike Mitzelfeld. Like David, Mike was a Chicago policeman and loved the EBT. Mike's interest in the EBT was no doubt sparked by his friend David Crement's passion for the road.



Here's a partially finished section of the layout depicting a non-prototypical connection with the McKelvey Logging Railroad at Rocky Ridge. In reality this connection occurred some miles from this spot, but limits in the size of the Crement basement forced a compromise. An, as yet unlettered, McKelvey shay is pulling a short train past the Rocky Ridge station and on to EBT rails. The depot, signal, shay's tender, wood hopper and caboose are all scratch built.

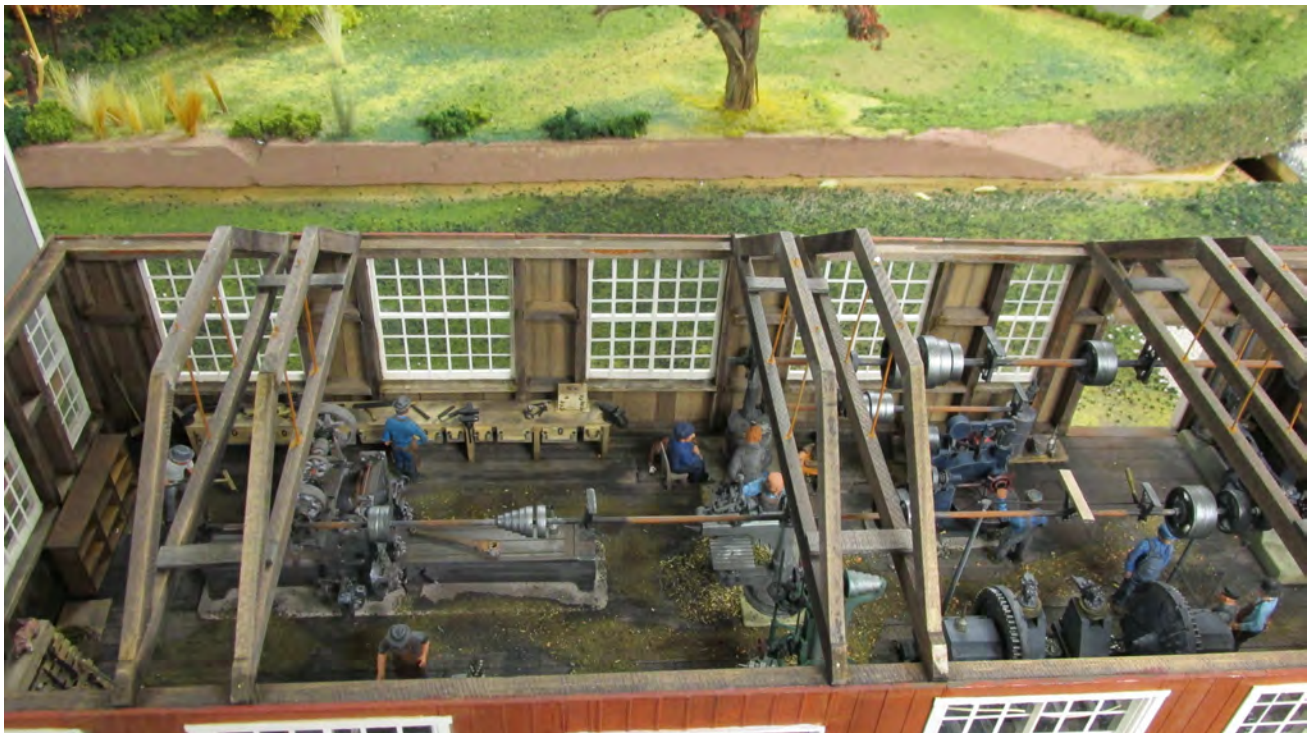


EBT #15 is at rest on the east side of Robertsdale. 2-8-2 Mikados were the preferred locomotive type of the EBT which operated its steam fleet in revenue service through April of 1956 when the mines and railroad shut down. Tourist operations with several EBT locos began in 1960 and continue today (though just barely). Wood box car #150 was built from a resin kit.

## David Crement's Layout of the Month con't



#18 pulls a loaded hopper train past the machine shop at Woodvale (above) MMR David Crement holds the removable roof of the Woodvale machine shop. (right) The roof and the rest of the shop were built from blueprints and photos of the actual structure. Below is the highly detailed interior of the shop including 15 different pieces of machinery. Except for the wide leather machine drive belts (which were not modeled to allow the roof to be removable), the shop's machinery, equipment, shelves, tools, etc. are as they were when the shop was in operation in 1939. David says, "There's no fiction on this railroad. There are abbreviations, compressions and a few compromises. But, everything here is built according to blue prints produced by the railroad, someone else, or me."



## David Crement's Layout of the Month con't

The Robertsdale section of the layout is seen on the upper deck and the Coles/Kimmel section on the lower deck in this photo. In total, there are four levels to the Crement layout. The back drop on the upper deck was painted by the late Mike Mitzelfeld, and he and David built the scenery behind and around Robertsdale. Below the Coles/Kimmel deck are horizontal file cabinets full of EBT blue prints, photos, drawings, notes and other valuable information. The layout's controls are housed in the rolling cabinet below the EBT herald to the far right. Three years of room preparation preceded the beginning of layout construction.



### Basic Layout Information

Layout Builder:	David Crement, MMR #410
Layout Location:	Carpentersville, IL
Layout scale:	On3
Layout Size:	14 x 36 feet
Layout Style:	Multi deck loops to loops; lowest deck height 34", highest deck height 61"
Layout theme/locale:	East Broad Top Railroad from Saltillo to Woodvale, Pennsylvania
Layout era:	Fall 1939
Construction started:	1994 with three years of room preparation before layout construction began
Bench work:	L girder cantilevered from the walls
Sub road bed/roadbed:	Sub roadbed: 3/8's inch, cabinet grade, nine ply plywood; Vinylbed road bed
Track:	450 total feet of hand laid, weathered, code 100 track was laid with a specially modified Kadee Spiker by David. Minimum radius is 45", maximum radius is 51". Hand laid #8 turnouts and surplus American Scientific timer motors with custom made turnout linkages are used throughout the layout.
Scenery:	Hydrocal plaster soaked paper base or poly fiber sections over chicken wire with Selkirk, Bob Walker, Woodland Scenics, and natural ground covers
Backdrop:	Painted directly on the plaster board walls by Mike Mitzelfeld
Structures:	98% scratch built from wood, styrene and/or metal from accurate plans; several structures feature extensive interior detailing
Locomotives:	RTR, kits or kit bashed to match specific EBT locos
Rolling Stock:	Scratch built, resin kits, craftsman kits to match specific EBT cars
Control:	Digitrax DCC
Favorite aspects of the hobby:	I enjoy scratch building everything including track, locomotives, cars, structures, and whatever parts are needed to make an accurate model.
Model RR influences:	My model railroading mentor, the late Charlie Martin, MMR #41, from Evanston.
Other:	David has done 35 years of research on the EBT and has drawers full of EBT photos, blue prints, drawings, plans, time tables, etc. He has produced 9 original scratch built models of EBT rolling stock and structures that have been developed into limited run kits. He is an expert craftsman in wood, styrene, and brass and other metals. David is also a skilled ship building modeler. He holds a tool and die certificate from Elgin Community College. A past Eagle Scout, David is active in Cub and Boy Scout activities.

## My Second Favorite Day of the Year by Walt Herrick

From the time I was eleven to about thirteen years old in the early 1960's, the Sunday before Thanksgiving was a special day of the year for me. It was on that day my dad would take me to the annual Valley Model Railroad Club's Open House in South Elgin. My younger brother Skip went with us a couple of times early on, but his interest in trains waned. Mine did too when I got into high school and high school basketball took over all of my fall and winter spare time and then some. But for four years, the trip to the Valley Model Railroad Club's HO layout was a highly anticipated event, second only to Christmas in my mind.



The Valley Model Railroad Club's Annual Open House is packed. According to the club's web site, this photo was probably taken at the 1962 Open House, a year the author would have also attended. Note the club's second story "control tower". Photo courtesy Valley Model Railroad Club.

I'd go to work on my dad early in November. "Can we go to the Elgin Model Train Open House again this year, dad? It's coming up in a couple of weeks, you know." I'd ask at dinner in front of the whole family hoping he'd say "yes" right away and I'd have witnesses. His reply was always the standard parental response, "We'll see." I knew if I pushed any harder, the response could escalate to, "Well, I don't know. Have you been keeping your room clean? How are your grades right now? Have you been getting your work done around the house?" So I simply said, "Thanks, dad. It would be great if we could go again," and began counting the days until the open house.

The ride over to the club was filled with high anticipation. We always went on Sunday. It was usually cold and we usually went in the late afternoon when the sun was low in the sky. South Elgin was only about a 35 minute drive from Crystal Lake, but for a kid, the ride seemed to take forever. The club was located in the old Chicago Aurora & Eastern electric line's Clintonville station out in the country in South Elgin. I thought it was the perfect place to have a model railroad club. When we finally pulled into the VMRR's gravel parking lot I was ready for trains. The lot was always pretty full even in the late afternoon near the end of the Open House.

The club's entrance was in the back of the station. As a kid I always thought it was curious that such a fabulous model railroad club had such a "small", plain single door entrance. In my kid view, the club deserved big double doors befitting the amazing layout displayed inside. In the early 1960's upon entering the club, you were greeted by the inside of a huge nine foot tall mountain. All you could see was the back of the mountain with its chicken wire, burlap and white plaster scenery base. Only a few random trains traveling through the mountain's tunnels could be seen from this vantage point. The layout room was typically packed and the aisles narrow. Getting out from behind "Doorway Mountain" could take several minutes, but the wait was worth it. As soon as you came out, the club's full array of trains, scenery, buildings, signals and track came into view. con't on page 7

## My Second Favorite Day of the Year con't

Track. I never dreamed there could be so much track on a model railroad! My little 5 x 9 foot American Flyer and HO layouts had a small fraction of the track the Valley Club had. There must have been a half dozen different long routes with electric switches (turnouts) and lighted, operating signals. Then there were the freight yards, industrial sidings, narrow gauge line, bridges, and trestles. And on all of this were running amazing trains of every shape and size. Steam, diesel, freight, passenger, and trolleys—they all were there. The back drop for all the railroading action was western scenery with the huge Doorway Mountain as the focal point. Locomotive servicing facilities, town scenes and industries added to the layout's magic. One year I got to go up into the second floor control tower of the Valley layout. It just about blew my little twelve year old mind. Guys running trains with head sets, 6 or 7 throttles and big control panels with hundreds of lights, it seemed. Now this was model railroading!

My time spent at the Valley Model Railroad Club's Open House always ended too soon. But I came home with a lot of inspiration to get me through the long winter months of kid model railroading. When I took my young grand sons to the Valley Club's Open House a few years ago, it was much the same as when I was twelve. Doorway Mountain was gone and the layout had been completely rebuilt. But, there were still those many trains, lighted signals, buildings, bridges, lots of track, and the operating tower. It was still the best way to spend the Sunday afternoon before Thanksgiving. (Today the Valley Model Railroad Club is still very active in the Clintonville depot. For information on the club, including dates for this year's Open House, go to their web site: [www.vmrr.org](http://www.vmrr.org).)

## Mike's Minute by Mike Hirvela

### Why I Like Trains

His question caught me by surprise. My 10 year old Grandson asked me, "Grandpa, why do you like trains?" I hadn't given it much thought really. Trains, real and model, have been a part of my life since I was 3 or 4 years old. So, I had to really think about my response to his question, in order to sound "Grandparently" and wise.

I grew up in a small city in Wisconsin, next to a warehouse for an aluminum company that made pots, pans, cookie sheets and coffee pots. The warehouse had a railroad siding. Every day I saw the C&NW switch engine pull loaded cars out and put empty cars for loading, back in the siding. The engine (an Alco S-1 or S-2 I found out later, usually the #1093) would put the loaded cars in the small yard and later take them in a train down the track to Manitowoc to be switched out to go to places all over the country. How they were routed and which direction they went and on whose tracks they traveled were a mystery. But I learned "how" by reading books and looking at maps in the local library. The more I found out, the more I liked.

And the train—"my" train—was something I saw every day and never grew tired of seeing. In fact, it was just the opposite—I looked *forward* to seeing my train. The engine and the train crew became like old friends—always there, always recognizing me and waving "hi". Early on, I had hopes and dreams of being an engineer on the engine of my train. Sadly, it was not to be. However, today I can be that engineer and the master of my own miniature railroad world in my basement, switching cars and routing them all over the country. Not surprisingly, one of my engine's is just like the 1093 of my youth.

So, why do I like trains? Because I do.

## Member News

Membership Records Trainmaster, Bert Lattan, reports that FVD membership in October was down 2 from September....Membership Promotions Trainmaster, Mike Hirvela and FVD member Jim Allen continue to man our NMRA promotions booth every other month at the DuPage Train Show. Thanks, guys!...Things are coming together quickly now for both the 2014 High Wheeler Train Show and Railfun Convention. Contact Jeff Jarr to help the division put on these two great events. Jeff's contact information is on page 11 of this newsletter....Those with Semaphore article ideas, photos, or desiring to have a hard copy of the Semaphore mailed to them should contact Walt Herrick. His contact information is on page 11.

## Working on My FVD Railroad: Crement EBT Blacksmith Shop

David Crement's Layout of the Month article in this newsletter doesn't show a structure project he has been working on since August. He'd hoped to have the structure, the East Broad Top Railroad's Rock Hill Furnace blacksmith shop, done in time for the Friends of the East Broad Top's Fall Reunion Columbus Day weekend, but painful back problems kept him out of his workshop for most of the several months before the event. David did make it to the FEBT Reunion with a partially completed blacksmith shop and it was well received even in its unfinished condition. The photos show why. It is another O scale "Crement Classic" in the making. From the photos you might think the model looks pretty complete. Not so. "There's about 400 more castings that need to go into the interior of this model before it's done," says David. The center piece of the interior is the Niles Bement Pound Company's huge steam operated forging hammer used to pound red hot metal into whatever shape the EBT needed. It is made of five separate lost wax castings produced by David. Castings were also made for a smaller drop hammer (4 castings) and a belt driven metal cutting saw (4 castings). So far models have also been made of 3 different hearths, a tempering oven, coal bunkers, a quenching tank, and more. All these for the *interior* of this structure, and there's 400 more detail parts to make and add! Many of us would "go nuts" trying to produce a model like this, but not David Crement. For him it's a labor of love. WH



This is MMR David Crement's East Broad Top Railroad's Rock Hill Furnace blacksmith shop in the making. The model is being built in O scale and is about 60% complete at this point with 400 more detail pieces needing to be added to the interior of the model. Most of these details are lost wax casting specially made by David. Says David, "The East Broad Top's restoration guys are some of the "nit pickiest" people you'd ever want to meet so your modeling has to be good! These guys will catch anything that's not right." The 16 scale foot tall Niles Bement Pound Company's steam operated forging hammer sits in the middle of the structure. Walt Herrick photos.



### Tip of the Month

When building a structure model that will have extensive interior detailing such as the David Crement machine shop and blacksmith shop models seen in this Semaphore, David recommends building the model "from the floor out" if at all possible. "Build the interior floor first so you can add your detailing 'out in the open'. Then add your walls and finally the roof," says David. David's advice makes a lot of sense. Doing it this way you don't have to squeeze your fingers around walls, roof supports, etc. when doing interior detailing. It will take a little more planning, but the modeling process will be easier, less frustrating, and the results will be worth it according to David. Good advice from one who's work speaks for itself, MMR David Crement. WH



## October's Contest

October's contest was "Caboose Train" with a limit of six cars. This was another popular category for division members drawing 8 entries and a total of 33 "caboose". Like last month, Jeff Jarr and Bob DiDomenico took first and third places. Unlike last month, Don Cook was our second place finisher. Big FVD "Congratulations!" go to our winners and to all those who entered. Now its on to "Passenger Cars" for the November contest. The limit is 4 cars. Contest photos by Jim Osborn.



FVD Superintendent, Jeff Jarr, is leading by example as he again places first in October's "Caboose Train" contest with 6 CNW cabooses. In September Jeff won our "Locomotives" contest.



MMR Don Cook's pristine Great Northern caboose placed second in October's "Caboose Train" contest.



Third Place went to Bob DiDomenico for his nice collection of 5 high rail, O scale, mixed road, cabooses. In September Bob also placed 3rd in the "Locomotives" contest.

## October's Clinic

Federal Railroad Administration Inspector, David Sima, presented October's excellent clinic on "Open Loads". Dave is an HO modeler as well as the FRA's inspector for northern Illinois, Wisconsin, and parts of Minnesota. He brought a bunch of photos taken on the job site for his work, more than two dozen of his own open load models, and a wealth of prototypical information and understanding about open loads. The Q&A's with Dave were excellent, and we especially thank him for making the long drive from his home in Burlington, WI to present a great clinic. In November MMR David Crement's clinic topic is, "Painting People". Jim Osborn photo.



## Check it out!

Have you checked out the *Railfun 2014* web site yet? Well, please do! The address is on page 11. It's got really good stuff. There are great photos of the 30 layouts that will be available for touring and operating, a schedule of the 17 clinics being offered and info on the other events taking place, too. Check it out and tell them Al and Winfred sent you.



## 2013 – 2014 Meeting Schedule

2013-2014 orange Meeting Schedule cards were mailed to members the last week in August. The complete schedule is also given below and on the FVD web site: [www.foxvalleydivision.org](http://www.foxvalleydivision.org). Any schedule changes or additions are posted on the FVD web site as soon as they are known.

<u>Date</u>	<u>Clinic</u>	<u>Contest</u>	<u>Other</u>
Nov. 17, 2013	Painting People by David Crement, MMR	Passenger Car(s) Limit 4 cars.	
Dec. 15, 2013	Module Scenery, Part 2 by Mike Wood	Tank Cars Limit 6 cars.	<b>1:00 pm start.</b> Lunch provided.
Jan. 19, 2014	Building Benchwork by Bill Kirchmeyer	“Rescue the Rolling Stock” Re-purposed rolling stock.	
Feb.16, 2014	TBA	Hopper Train Limit 6 cars.	
<b>March 1, 2, 2014</b>	<b>High Wheeler 2014</b> Train Show at Harper College in Palatine Sponsored by the FVD. 9 am to 5 pm each day. <u>FVD volunteers needed.</u> Contact Jeff Jarr at <a href="mailto:jjarr@comcast.net">jjarr@comcast.net</a> or 773-286-8755.		
April 13, 2014	TBA	Diorama	<b>1:00 pm start.</b> Lunch provided. FVD officer elections.
<b>May 16, 17, 18, 2014</b>	<b>Rail Fun 2014</b> Mid West Region Convention hosted by the FVD In Schaumburg, IL. <u>FVD volunteers needed.</u> Contact Jeff Jarr at <a href="mailto:jjarr@comcast.net">jjarr@comcast.net</a> or 773-286-8755.		
<b>Note:</b> <u>there will be no May FVD monthly meeting in lieu of the Rail Fun convention being hosted by the FVD.</u>			
<b>July 13 - 19, 2014</b>	<b>NMRA National Convention in Cleveland, Ohio.</b> Check the national web site <a href="http://www.nmra.org">www.nmra.org</a> and <i>NMRA Magazine</i> for further information.		

## NMRA Web Sites

<b>Fox Valley Division</b>	<a href="http://www.foxvalleydivision.org">www.foxvalleydivision.org</a>
<b>High Wheeler 2014</b>	<a href="http://www.highwheelertrainshow.com">www.highwheelertrainshow.com</a>
<b>Railfun 2014</b>	<a href="http://www.foxvalleydivision.org/2014railfun/welcome.html">www.foxvalleydivision.org/2014railfun/welcome.html</a>
<b>Midwest Region</b>	<a href="http://www.mwr-nmra.org">www.mwr-nmra.org</a>
<b>National</b>	<a href="http://www..nmra.org">www..nmra.org</a>
<b>2014 National Convention</b>	<a href="http://www.2014cleveland.org">www.2014cleveland.org</a>

## Fox Valley Division Board Contact Information

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*Do you have a question or two? Visit our website at: [www.foxvalleydivision.org](http://www.foxvalleydivision.org).  
Or, call or email one of us above. We're here to help you!*

### About the Fox Valley Division

If you receive this newsletter you live in the Midwest Region and Fox Valley Division of the National Model Railroad Association or NMRA. The Fox Valley Division (FVD) includes all of McHenry and parts of Cook, Kane, and Lake Counties in northeast Illinois. About 240 members of varied ages and modeling ability levels belong to the FVD. Almost all modeling scales are represented in our division. Membership in the division is free as are the Semaphore newsletters.

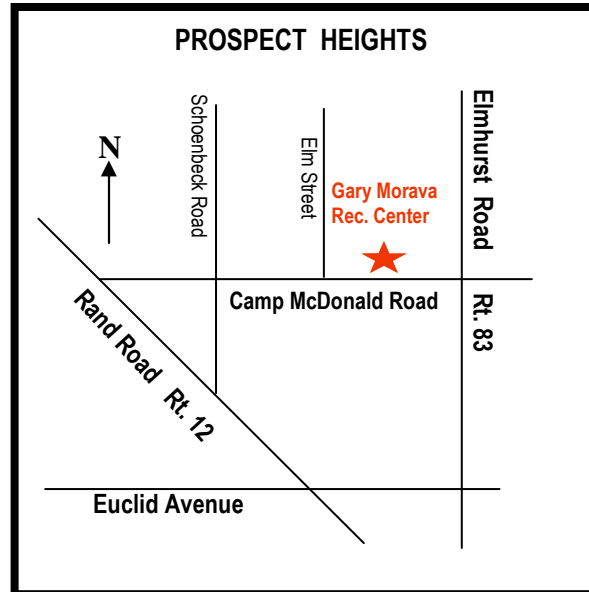
FVD general meetings are held September through May at the Gary Morava Center in Prospect Heights from 1:30 to about 4:00 pm. Each meeting features a clinic, model contest, information of interest to the membership, and a time for socializing. Outings, layout tours, and operating sessions are also held periodically in addition to, or instead of, the general meetings. To promote the hobby of model railroading, the FVD sponsors and runs the large train show, "High Wheeler", in early March each year at Harper College in Palatine, Illinois.

We encourage you to take advantage of all the FVD has to offer. We are here to answer your questions, help improve your modeling, and help you better enjoy the great hobby of model railroading. Join us at a FVD meeting or outing. We'd love to see you.

Visit us on the web at:

[www.foxvalleydivision.org](http://www.foxvalleydivision.org).

**Questions? Contact FVD Superintendent, Jeff Jarr, or any FVD Board member listed on the previous page of this newsletter. We are here to help you!**



**Regular Meeting Location: Gary Morava Center, 110 Camp McDonald Road, Prospect Heights, IL**

**Regular Meeting Time: 1:30 to 4:00 p.m. See page 10 for this year's meeting schedule.**



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